



Airlift Operations Part I

612-501-02



References



DOD 4500.0-R, DTR, Part III, *Mobility*

FM 55-9, *Unit Air Movement Planning*

FM 55-65, *Strategic Deployment*

FORSCOM/ARNG Reg 55-1, *Unit Movement Planning*



Aerial Movement



- Most units will ship their equipment & supplies via ocean vessels to SPOD & airlift unit personnel to an APOD
- Selected units (airborne, light forces) plan for deploying both equipment & personnel by airlift.



Deployment Authority

- The decision to deploy military forces originates with the NCA
- The CJCS issues the deployment order
- Deployment order addressed to appropriate Unified Commands and the Services



USTRANSCOM



- USTRANSCOM provides global land, sea & air transport to deploy and sustain forces



HQ USTRANSCOM
Scott AFB, IL





Air Mobility Command (AMC)



- AMC is USTRANSCOM component command that provides strategic airlift

AMC uses organic and commercial airlift assets

AMC's Tanker Airlift Control Element (TALCE) coordinates loading & off-loading operations at aerial ports





Deploying Unit Movement Instructions



- Deploying unit's higher headquarters issues movement orders/instructions based on CJCS & MACOM deployment orders/directives:
- Guidance may include:
 - Date/times for movement from HS to APOE
 - Equipment to deploy
 - Special logistical & soldier support instructions



UMC -- Air Movement Responsibilities

- UMC

Primary installation POC for airlift operations and coordinating airlift requests
Maintains coordination with deploying unit, Army MACOM or ASCC, & AMC POC for aircraft departure times and mission changes
Coordinates (along with ITO) installation support for movement to and activities at APOE



Unit Preparation for Air Movement





UMO -- General Responsibilities



- UMO:
 - Coordinates unit airlift planning and preparation activities
 - + Includes coordination with higher headquarters & UMC for unit support & procedures during movement to and processing at APOE.



Unit & UMO Preparation Tasks



- Unit has multiple planning & preparation tasks
- Technical assistance available from higher HQ, installation UMC, A/DACG & TALCE
- Unit tasks include:
Prepare/review air movement plan with higher HQ. Plan should detail unit actions and include sequence of movement for troops & equipment.



Unit & UMO Preparation Tasks (Cont)

Establishing unit priorities/sequence for arriving at APOD or area of operations

- Planning movement to POE (convoy, rail, water, commercial truck)

Preparing movement documentation (vehicle load plans, DEL, HAZMAT)



Unit & UMO Preparation Tasks

- Determining requirements for 463L pallets and associated pallet top/side nets & plastic covers
- Obtaining BBPCT and aircraft shoring & floor materials
- Preparing & organizing soldiers for air movement
- + Includes designating key personnel, weapons procedures, aircraft safety & manifesting



Unit & UMO Preparation Tasks (Cont)

Preparing equipment & cargo to include
463L pallet & vehicle loads

- + Configure equipment for air

- + ~~movement~~ Weigh vehicles and mark center of
balance

Ensuring documentation & load plans are
complete.

Providing load team training (if required)



Preparing Personnel and Equipment for Air Movement



Preparing Personnel for Air Movement



- Identify key unit personnel and assign duties & responsibilities
- Key positions include:

Unit liaison to A/DACG

- + Facilitates communication between unit and A/DACG
- + Clarifies processing procedures and resolves problems



Preparing Personnel for Air Movement (Cont)

- Key positions (Cont)

Planeload or troop commander:

- + Assumes control of all passengers listed for movement on the flight
- + Ensures passengers are briefed on aircraft procedures
- + Ensures necessary support is provided during en route stops



Preparing Personnel for Air Movement (Cont)

- Key personnel training:

Unit vehicle drivers & equipment operators may require training in:

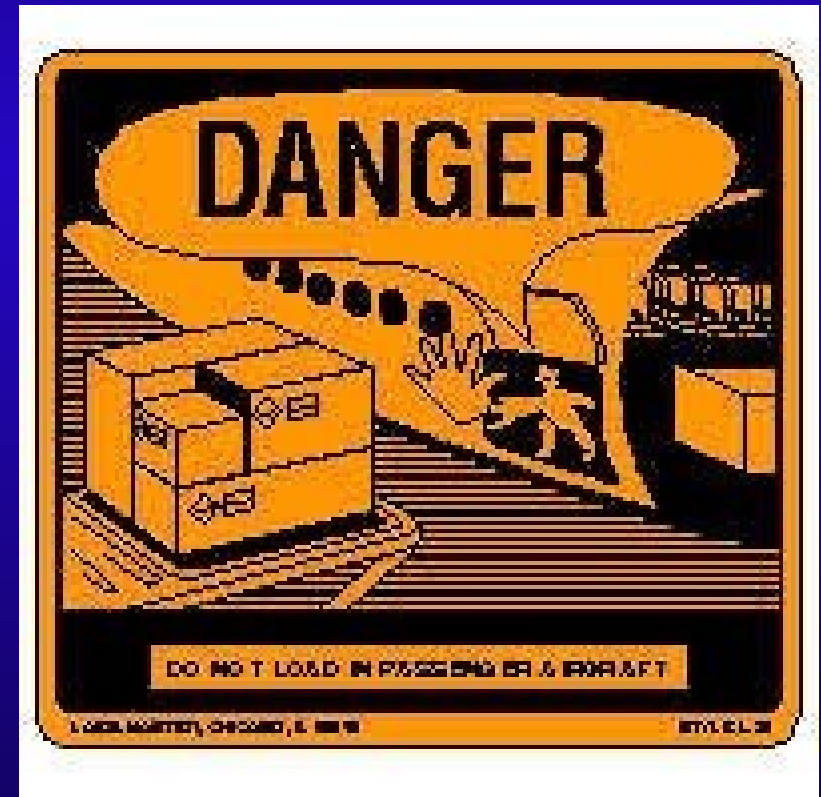
- + Aircraft loading & off-loading and proper procedures for restraining unit cargo (under aircraft load master supervision)



Preparing Personnel for Air Movement (Cont)

- Key personnel training (Con't)

Personnel
preparing
hazardous cargo
for air movement
require training &
certification





Preparing Personnel for Air Movement

- Develop and brief (Continual) weapons & ammunition procedures for airlift

oPS Reference TM 38-250 for instructions on packing & certification of ammunition
Weapons should be "cleared" before boarding aircraft
Personnel requiring loaded weapons must be identified to aircraft commander



Preparing Personnel for Air Movement (Cont)

- Brief personnel

Briefing should provide a basic understanding of in-flight responsibilities and procedures for disembarking aircraft.

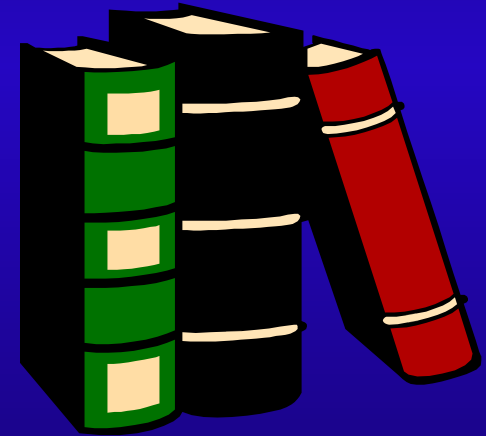
Briefing should include identification of key personnel (troop commander, load master)



Preparing Equipment & Cargo for Air Movement



- References for equipment preparation include:
 - FM 55-9, *Unit Air Movement Planning*, Appendix B
 - FM 55-65, *Strategic Deployment*, Appendix C
 - FORSCOM/ARNG 55-1, *Unit Movement Planning*, Chapter 5
 - DD Form 2133, *Joint Airlift Inspection Record*





Preparing Equipment & Cargo for Air Movement (Cont)

- Preparing equipment & cargo includes:
 - Loading pallets & vehicles
 - Preparing vehicles
 - Weighing & marking vehicles & pallets
 - Manifesting
 - Preparing aircraft load plans

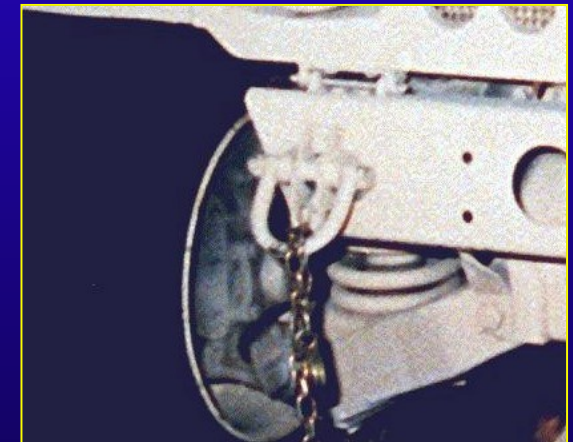




Preparing Equipment & Cargo for Air Movement

(Cont)

- Preparing equipment & cargo (Cont):
 - Ensuring vehicles & equipment are serviceable
 - Ensuring equipment lifting or tie-down shackles & devices are in position





Preparing Equipment & Cargo for Air Movement

- Vehicles, self-propelled equipment, and engine powered ground equipment will not be transported with fuel tanks more than $\frac{1}{2}$ full
 - Self propelled vehicles may be up to $\frac{3}{4}$ full for selected deployment (e.g., opposed entry operations) Check with higher hqs & A/DACG
 - Bulk fuel servicing trucks and trailers must be drained or purged



Preparing Equipment & Cargo for Air Movement (Cont)

- All Vehicles, equipment and other cargo will be inspected prior to loading aboard aircraft
 - DD Form 2133, *Joint Inspection Airlift Record* used by TALCE.
 - Equipment checked for serviceable engine and brakes, fluid leaks, proper fuel levels, and secured battery



Load Pallets & Vehicles



- Deploying unit ensures pallets & vehicles are properly loaded for air movement:
 - Hazardous cargo must be prepared & loaded IAW TM 38-250 (AFJM 24-204)
 - 463L pallets are built with unit cargo, covered, and then restrained on the pallet using cargo nets



Load Pallets & Vehicles (Con't)



- Supplies & equipment are loaded on vehicles IAW with unit's vehicle load plans & firmly secured to prevent movement
 - Shifting cargo presents an unsafe condition for aircraft flight
 - Vehicle loads must not exceed rated cross-country capability for the vehicle



SUMMARY



On Learnin g



On Learnin g



QUESTION 1: What reference documents provide guidance for preparing unit HAZMAT for movement by air?

Answer 1: TM 38-250, *Preparing Hazardous Materials for Military Air Shipments*, and the *International Air Transport Association - Dangerous Goods Regulation*



On Learnin g



QUESTION 2: What installation staff position is the unit's primary point of contact for airlift operations?

Answer 2: The Installation's Unit Movement Coordinator (UMC)



Aircraft Load Planning



Aircraft Load Planning

- Deployment planners and deploying unit follow 2 basic steps in developing load plans:
 - Step 1: Identify the total number of personnel, amount of equipment & general sequence of movement
 - Step 2: Design loads for each aircraft



Aircraft Load Planning (Cont)



Information gathered in Step 1 is documented on the following preliminary load planning tools:

- + DD Form 2327, Unit Aircraft Utilization Plan - Used to estimate the number & types of aircraft that will be needed
- + DD Form 2328 - Summarizes the data from DD Form 2327



Aircraft Load Planning -- General Guidance



- Use DOD 4500.9-R, FM 55-9, & FORSCOM/ARNG 55-1
- General guidance & considerations:
 - Position personnel, equipment and supplies to facilitate off-load
 - + Avoid floor-loaded cargo on aircraft planned for rolling stock



Aircraft Load Planning -- General Guidance (Cont)

Positioning personnel, equipment
and supplies (Cont)

- + Load vehicles facing the exit ramp
- + Load trailers with their prime mover
- + Place palletized cargo aft of rolling stock & passengers



Aircraft Load Planning -- General Guidance

(Cont)

Plan for the use of C-141 or C-17 aircraft for majority of strategic movement of unit equipment & cargo





Aircraft Load Planning -- General Guidance (Cont)



C-5
aircraft
are limited
assets
best used
for outsize
items





Aircraft Load Planning -- General Guidance

C-130 aircraft (Cont)

are primarily
used for
tactical airlift
operations





Aircraft Load Planning -- General Guidance

(Cont.)

Identify a minimum of 2 passengers with each load of equipment or cargo to act as custodians if aircraft should be diverted & cargo down-loaded somewhere other than original destination

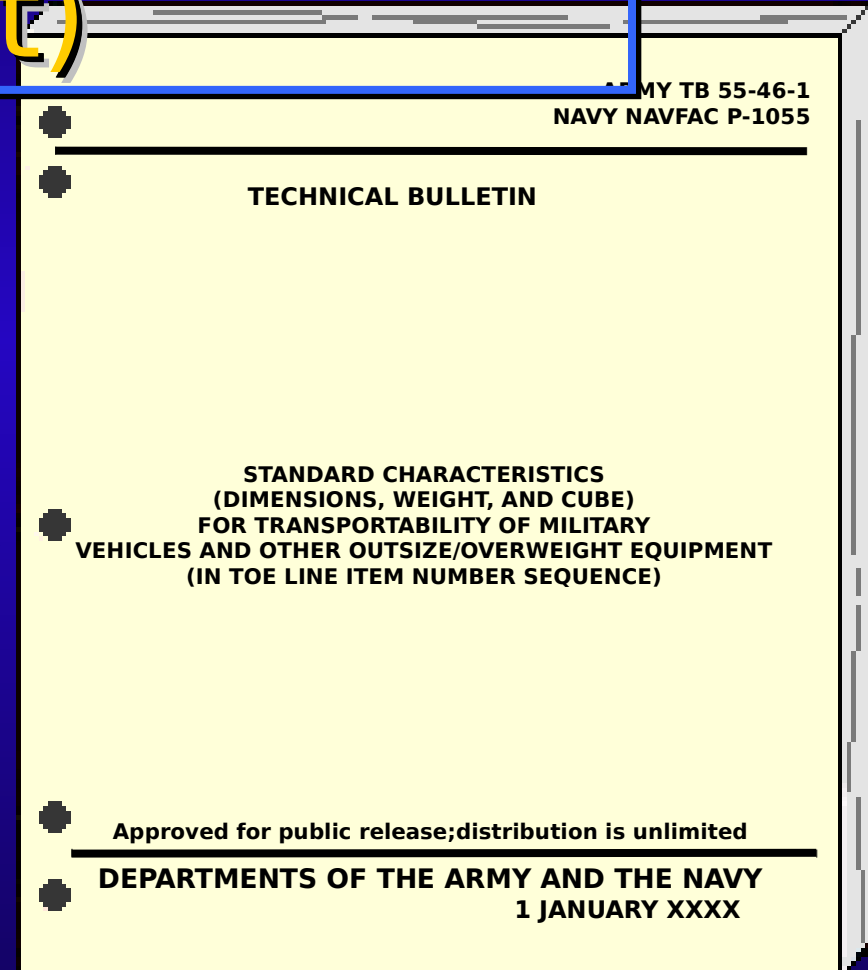
- + Qualified operators should be transported with self-propelled vehicles



Aircraft Load Planning -- General Guidance

Refer to TB 55-46-1
Standard (Cont)

*Characteristics for
Transportability of
Military Vehicles &
Other Outsize /
Overweight Equipment*
for initial equipment
configuration &
dimensional planning
information

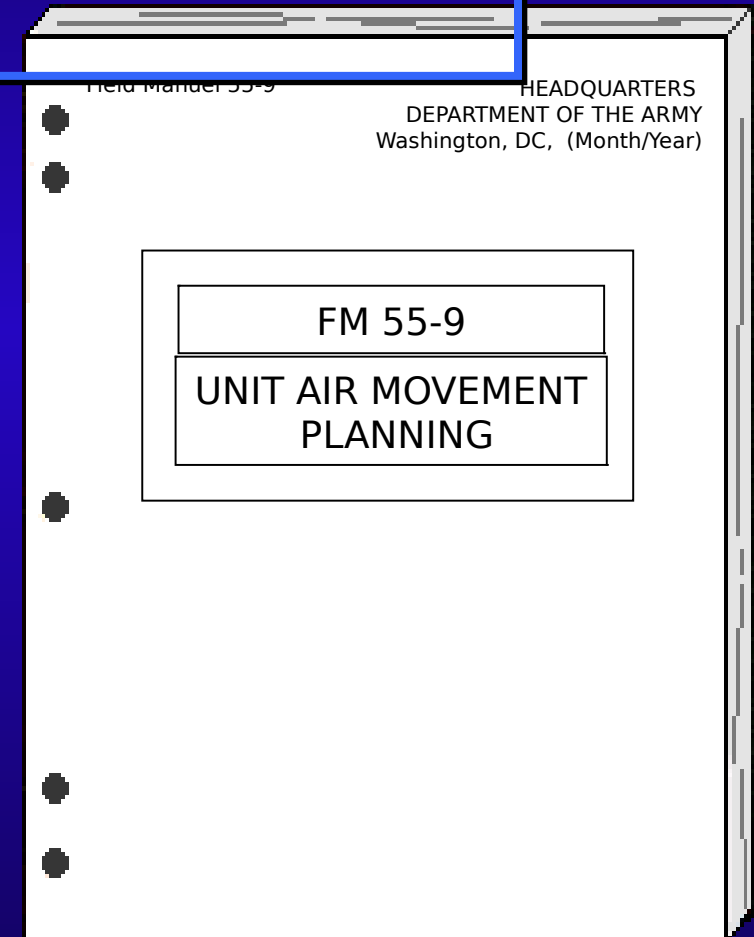




Aircraft Load Planning -- General Guidance

Reference FM (Cont)

ensure pallet weight, axle loads, wheel loads, tire footprint loads & general floor loads conform to the fuselage zone, compartment & loading limitations for the aircraft





Aircraft Load Planning -- Load Shoring



- Unit might have to provide shoring when shipping some vehicles/equipment by air
- General guidance & considerations:
 - Rolling shoring protects aircraft floor when loading equipment
 - Parking shoring protects floor from concentrated loads
 - Sleeper shoring is used under the axle or frame of equipment



Aircraft Load Planning -- Load Shoring (Cont)



- General guidance & considerations (Cont):
 - If shipping tracked vehicles usually must use rolling shoring and parking shoring
 - + Tracked vehicles could deploy with new rubber pads but redeploy with worn pads & need shoring
 - If shipping large pneumatic tired vehicles & equipment, must use sleeper shoring



Aircraft Load Plans



Identify aircraft allowable cabin load.
Seek assistance from higher HQ and UMC early in planning to obtain accurate information on aircraft cabin load for specific deployments

Goal is to develop load plans that support unit mission while maximizing aircraft utilization



Aircraft Load Plans (Cont)



Cross-loading of like capabilities helps prevent total loss of capability due to aircraft diversion or delay



Example: If all of a deploying unit's communications equipment is loaded on a single aircraft, & that aircraft fails to reach the mission location, the unit's communications capability is severely impacted



Manifests

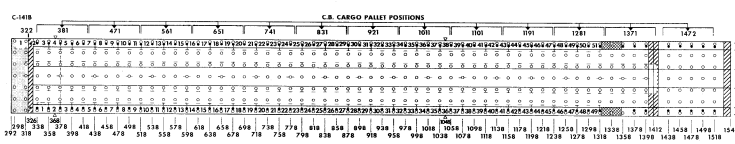
- DD Form 2328 takes the data from DD Form 2327 & summarizes it to identify quantities & type of aircraft planned for the air move
- Once loads have been designed for each aircraft, prepare:
 - DD Form 2130 Cargo Manifest
 - DD Form 2131 Passenger Manifest



Manifests (Cont)

- DD Form 2130
Cargo Manifest

Lists all
cargo
loaded on
aircraft

| 1. UNIT BEING AIRLIFTED (Name or Number) | 2. UNIT IDENTIFICATION CODE | 3. TYPE MOVEMENT PLAN | 4. MOVEMENT DATE | 5. UNIT AIRCRAFT LOAD NO. OF | PAGE | OF | PAGES |
|--|---------------------------------------|-----------------------|---------------------------|---------------------------------|------|----|-------|
| 6. MISSION NUMBER | 7. AIRCRAFT SERIAL NUMBER (Last Five) | 8. CONFIGURATION | 9. DEPARTURE AIRFIELD/DTG | 10. DESTINATION AIRFIELD/DTG | | | |
| 11. ACTUAL LOADOUT | | | | | | | |
| SCALE: 1/4 INCH = 3 FEET | | | | | | | |
| <div><div>C-141B</div><div>C.B. CARGO PALLET POSITIONS</div><div></div><div>REL 01.5 REL 10.0 REL 16.0 REL 18.0 REL 21.0 REL 24.0 REL 27.0 REL 30.0 REL 33.0 REL 36.0 REL 39.0 REL 42.0 REL 45.0 REL 48.0 REL 51.0 REL 54.0 REL 57.0 REL 60.0 REL 63.0 REL 66.0 REL 69.0 REL 72.0 REL 75.0 REL 78.0 REL 81.0 REL 84.0 REL 87.0 REL 90.0 REL 93.0 REL 96.0 REL 99.0 REL 102.0 REL 105.0 REL 108.0 REL 111.0 REL 114.0 REL 117.0 REL 120.0 REL 123.0 REL 126.0 REL 129.0 REL 132.0 REL 135.0 REL 138.0 REL 141.0 REL 144.0 REL 147.0 REL 150.0 REL 153.0 REL 156.0 REL 159.0 REL 162.0 REL 165.0 REL 168.0 REL 171.0 REL 174.0 REL 177.0 REL 180.0 REL 183.0 REL 186.0 REL 189.0 REL 192.0 REL 195.0 REL 198.0 REL 201.0 REL 204.0 REL 207.0 REL 210.0 REL 213.0 REL 216.0 REL 219.0 REL 222.0 REL 225.0 REL 228.0 REL 231.0 REL 234.0 REL 237.0 REL 240.0 REL 243.0 REL 246.0 REL 249.0 REL 252.0 REL 255.0 REL 258.0 REL 261.0 REL 264.0 REL 267.0 REL 270.0 REL 273.0 REL 276.0 REL 279.0 REL 282.0 REL 285.0 REL 288.0 REL 291.0 REL 294.0 REL 297.0 REL 300.0 REL 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SUMMARY



On Learnin g



On Learnin



QUESTION 1: What reference can be used to determine the type of aircraft specific unit equipment is certified for movement on?

Answer 1: TB 55-46-1, Standard Characteristics for Transportability of Military Vehicles and Other Outsize / Overweight Equipment.



On Learnin



QUESTION 2: What forms are used to manifest passengers and cargo deploying by air?

Answer 2: DD Form 2130 series & DD Form 2131.



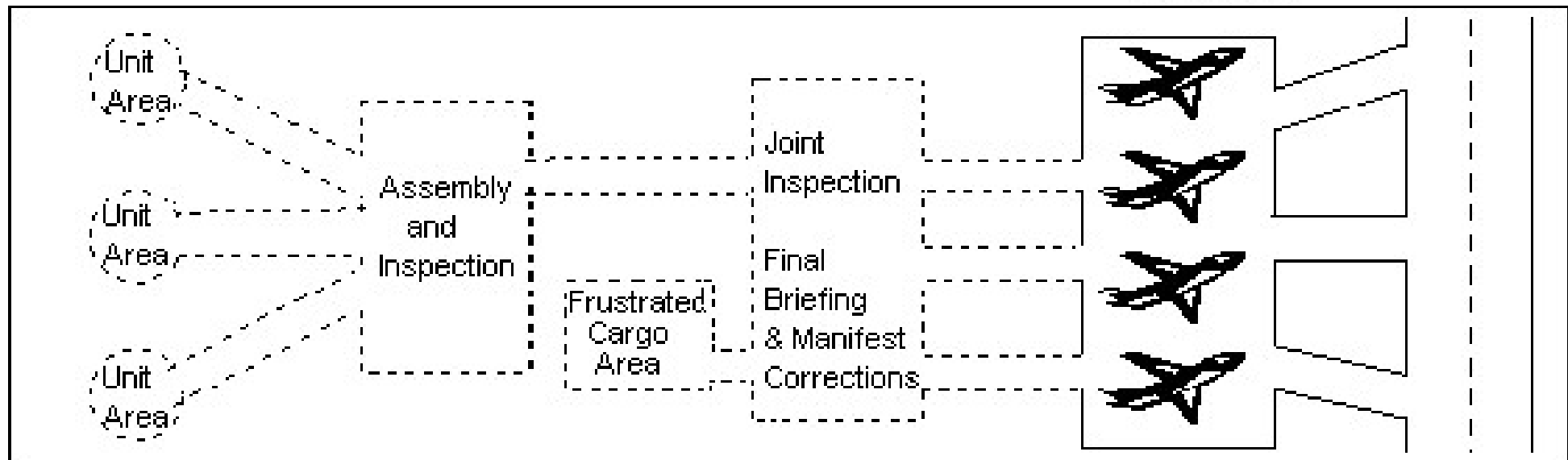
Aerial Port of Embarkation Operations



Notional APOE

- Aerial Port of Embarkation functional

Marshaling Area Alert Holding Area Call Forward Area Ready Line/Loading Ramp Area

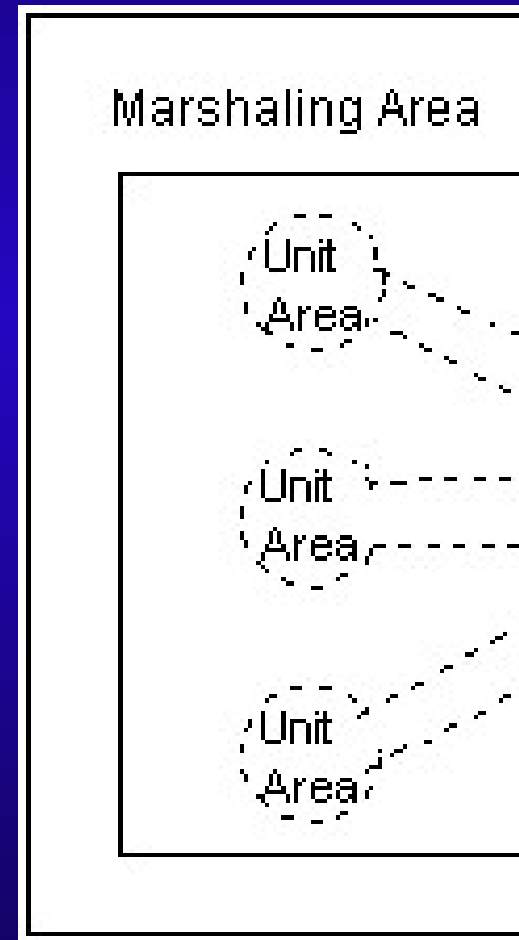




Notional APOE -- Marshaling Area



- Marshaling area
 - Deploying unit area of responsibility
 - Unit conducts final preparations for air movement





Marshaling Area Responsibilities



- Deploying units responsibilities include:
 - Establish liaison with A/DACG
 - Perform preparation of vehicles & equipment
 - Prepare passenger & cargo manifests
 - Assemble personnel, supplies & equipment into plane loads (chalks)
 - Ensure planeload/troop commanders briefed & escorts assigned (if required)



Marshaling Area Responsibilities (Cont)

- A/DACG responsibilities include:
 - Establish liaison with unit & TALCE
 - Coordinate with TALCE for USAF technical assistance for deploying unit
 - Call unit chawks forward to Alert Holding Area



Notional APOE --Alert Holding

• Alert Holding Area

A/DACG area of responsibility

- Equipment, vehicle and passenger control area

Equipment & documentation is pre-inspected

Alert Holding Area

Assembly
and
Inspection



Alert Holding Area Responsibilities



- Deploying units responsibilities include:
 - Ensure chawks arrive in Alert Holding Area when scheduled
 - Provide A/DACG with passenger/cargo manifests
 - Correct any unit chalk discrepancies
 - Pass control of unit chawks to A/DACG



Alert Holding Area Responsibilities

(Cont)

- A/DACG responsibilities include:
 - Ensure the unit loads arrive on time
 - Receive & inventory unit chawks
 - Inspect chawks for completeness
 - Verify accuracy of weight & center of balance
 - Establish a discrepancy correction area



Alert Holding Area Responsibilities

(Cont)

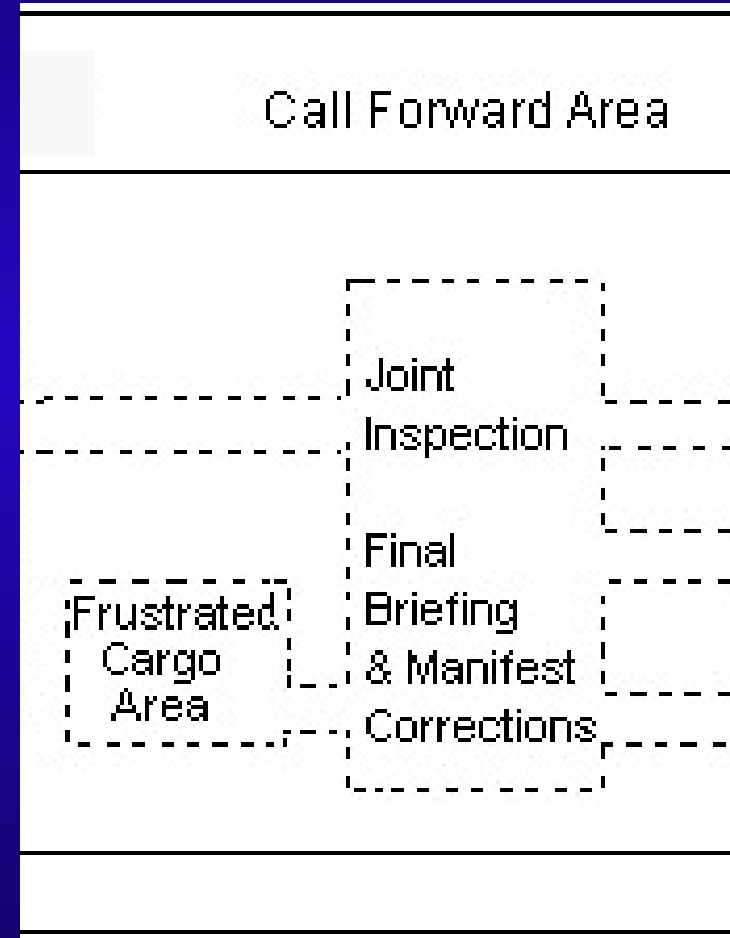
- A/DACG responsibilities include: (Cont)
 - Inspect documentation for accuracy & completeness
 - Provide limited maintenance, POL & related services as needed
 - Coordinate MHE support
 - Direct chinks to the joint inspection area (call forward area)



Notional APOE -- Call Forward Area



- Call Forward Area
 - Dual area of responsibility between A/DACG & TALCE
 - Used for joint inspection of deploying equipment & documentation





Call Forward Area Responsibilities



- A/DACG responsibilities:
 - Maintain communications with unit & TALCE
 - Ensure unit passenger/cargo manifests are correct
 - Ensure all time tables are met
 - In coordination with deploying unit, ensure all discrepancies identified during joint inspection with TALCE are corrected



Call Forward Area Responsibilities (Cont)

- A/DACG responsibilities: (Cont)
 - Provide load team personnel & support equipment (safety, MHE, pusher vehicle)
 - Ensure load team members properly outfitted
 - Escort unit chalks to ready line & ensure all unit personnel are briefed on flight line safety
 - Provide limited logistics support (defueling, maintenance) for deploying units



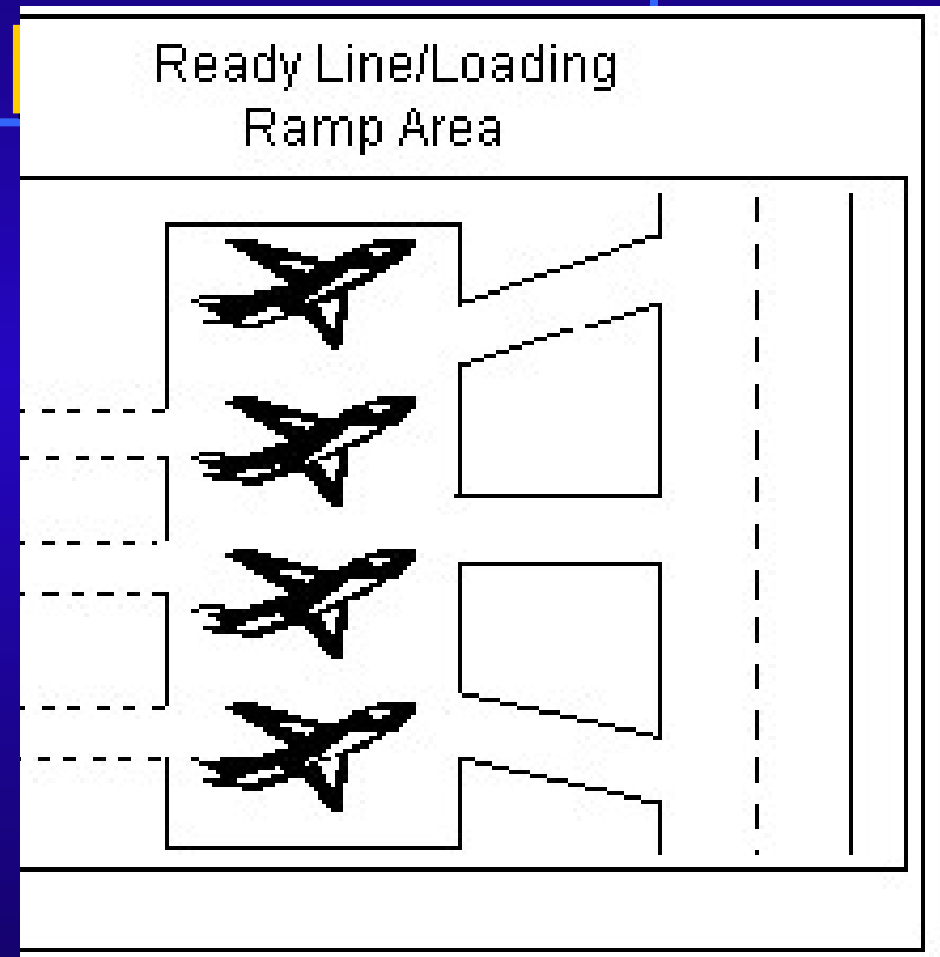
Call Forward Area Responsibilities

- TALCE responsibilities include:
 - Coordinate with A/DACG on any changes due to aircraft configuration
 - Conduct joint inspection with unit & A/DACG
 - Provide passenger briefing for on/offload procedures and for flight line safety
 - Provide team chief for aircraft load team
 - Notify A/DACG when to dispatch unit chalks to the loading ramp/ready line area



Notional APOE -- Ready Line/Loading Ramp

- Ready Line/
Loading Ramp Area
TALCE area of
responsibility
- TALCE controls
all
passengers/carg
o
& stages chawks
for aircraft





Ready Line / Loading Ramp Responsibilities

- Planeload or troop Commander responsibilities
 - Controls aircraft passengers
 - Retains copy of final passenger/cargo manifest
 - Provide unit assistance, as required, to assist load master in securing vehicles
 - Ensures vehicle drivers follow loadmaster instructions for loading equipment



Ready Line / Loading Ramp Responsibilities (Cont)

- A/DACG responsibilities
 - Transfers control of aircraft loads to the TALCE
 - Provides load teams to assist in loading aircraft
 - Maintains coordination with the deploying unit representative and the TALCE



Ready Line / Loading Ramp Responsibilities (Cont)

- TALCE responsibilities:
 - Accept chawks from A/DACG & load aircraft
 - Ensure each chawk is positioned to its aircraft
 - Ensure all drivers briefed on flight line safety
 - Maintain liaison with Air crew and A/DACG
 - Maintain communications with unit & A/DACG



Ready Line / Loading Ramp Responsibilities (Cont)

- TALCE responsibilities (Cont):
 - Coordinate with aircraft primary loadmaster & ensure loads are placed aboard aircraft on time
 - Provide & operate MHE, if required
 - Escort passengers to aircraft
 - Provide primary loadmaster with required copies of passenger/cargo manifests



Summary



On Learnin g



On Learnin g

QUESTION 1: What APOE area is used by the deploying unit to make final equipment preparations?

Answer 1: Unit Marshaling Area



On Learnin g

QUESTION 2: What APOE area is used for transferring control of unit aircraft loads to the A/DACG?

Answer 2: The Alert Holding Area



Let's
Review



On
Review



On Review

QUESTION 1: What APOE area is used for the joint inspection of deploying unit equipment?

Answer 1: The Call Forward Area.



On Review

QUESTION 2: What are the two primary types of aircraft that the unit uses when planning strategic airlift movements ?

Answer 2: The C-141 and the C-17.



On Review

QUESTION 3: True or False. TB 55-46-1 equipment weights are acceptable for final aircraft cargo manifests?

Answer 3: False: Actual measured dimensions and weights must be used when submitting final manifests.



On Review

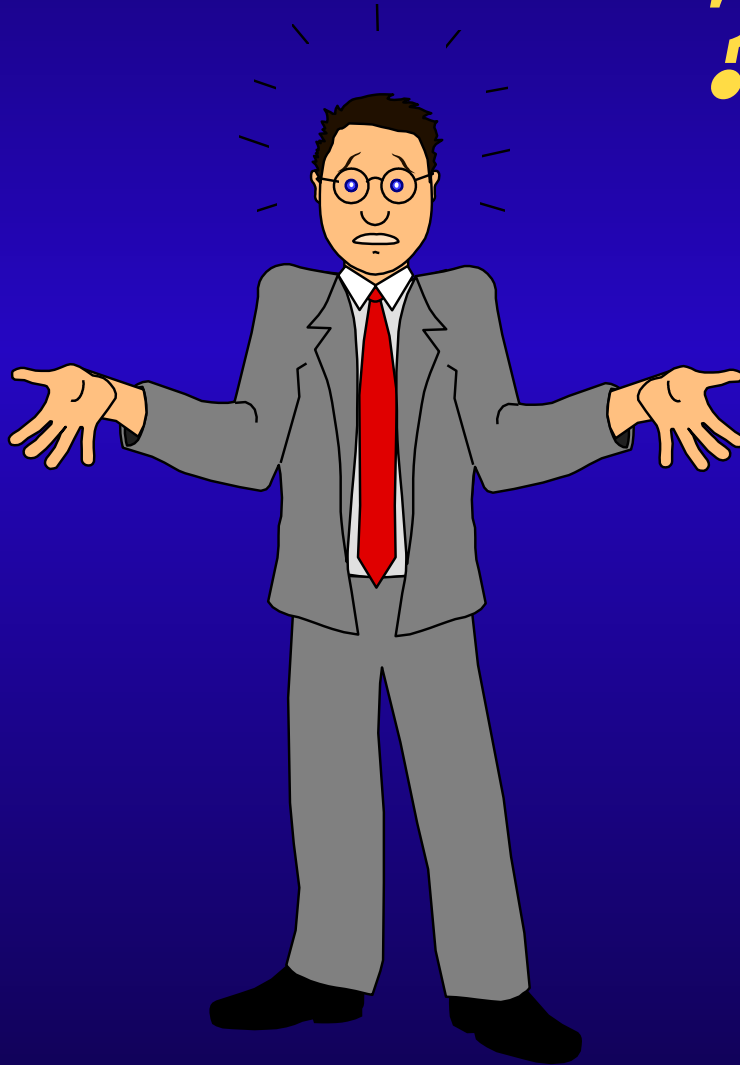
QUESTION 4: What organization at the APOE ensures that aircraft passengers are properly briefed on aircraft safety, and delivers the passengers to the aircraft?

Answer 4: The TALCE briefs passengers and escorts them to the aircraft.



QUESTIONS

???





What's Coming



UMODPC



Break



10 mins